

SCANNED  
JAN 18 2002  
BY TRC

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
BUREAU OF HIGHWAYS

REV. 1-5-82 : FEDERAL AID PROJECT NO.  
REVISED ON THIS PROJECT.

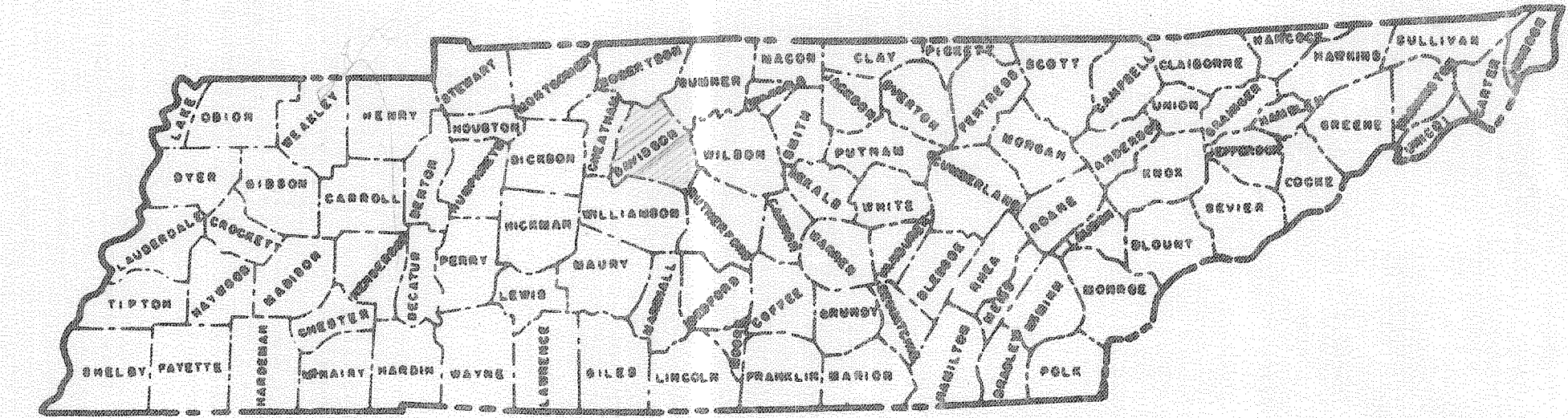
FED. ROAD DIST. NO.	T E N N	YEAR	SHEET NO.
3		1981	1
FED. AID PROJ. NO.	ID-440-4 (45) 212		
STATE PROJ. NO.	19015-3111-44		

DAVIDSON COUNTY

BRIDGE AND APPROACHES AT L & N R.R.  
(OVER I-440 AND RAMP 'N-F')

BRIDGE AND APPROACHES

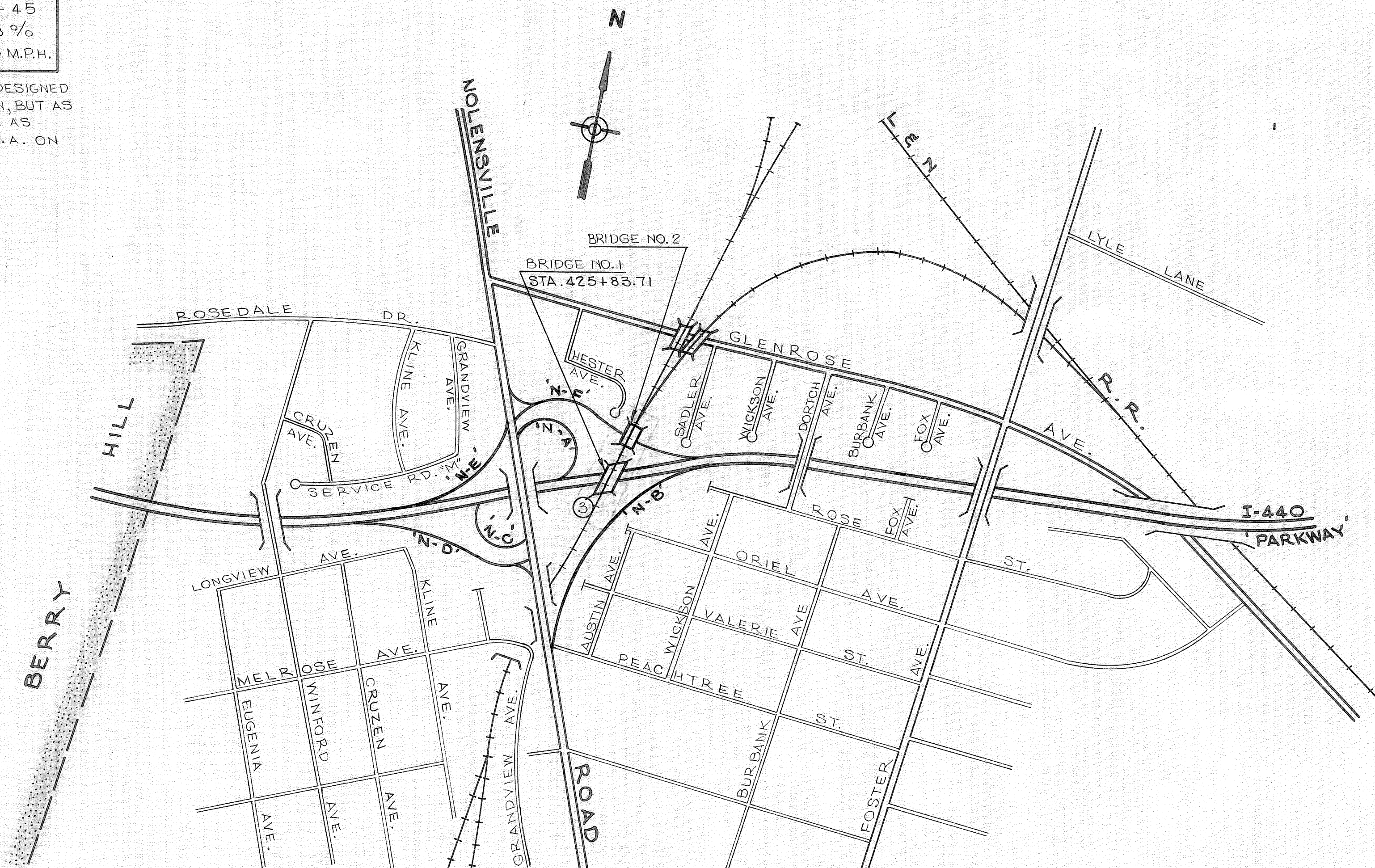
INTER STATE HIGHWAY NO. I-440 F.A.H.S. NO. I-440  
'PARKWAY'



TRAFFIC DATA	
A.D. T. (1981)	55,000
A.D. T. (2001)	64,000
D. H. V. (2001)	6,400
D	55-45
T	8%
V	60 M.P.H.

THIS PROJECT WAS NOT DESIGNED BASED ON TRAFFIC SHOWN, BUT AS DESCRIBED IN THE F.E.I.S. AS APPROVED BY THE F.H.W.A. ON 5-19-80.

NO EQUATIONS  
NO EXCLUSIONS



SPECIAL NOTE

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

ROADWAY LENGTH	0.00	MILES
BRIDGE LENGTH	0.00	MILES
PROJECT LENGTH	0.00	MILES

\* BRIDGES ARE ON L & N RAILROAD;  
L & N R.R. OVER I-440 (.042 MI.)  
L & N R.R. OVER RAMP 'N-F' (.016 MI.)

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION FORMERLY DEPARTMENT OF HIGHWAYS DATED MARCH 1, 1981 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

DESIGNER REAVIS PENDERGRASS

CHECK BY \_\_\_\_\_

APPROVED *Lewis Evans*  
STATE TRANSPORTATION ENGINEER

DATE \_\_\_\_\_

APPROVED *Paul E. F...*  
COMMISSIONER

U.S. DEPARTMENT OF TRANSPORTATION  
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: \_\_\_\_\_

DIVISION ADMINISTRATOR

DATE \_\_\_\_\_



TENN. STATE HIGHWAYS  
 Plans  
 Divn. No. 27

SCANNED

SCANNED  
 BY TRC

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	1981	I-440-4(45)212	1A

**SHEET NO. SHEET NAME**

- 1 ..... TITLE SHEET
- 1A ..... INDEX
- 2 ..... TYPICAL SECTIONS AND DETAILS
- 2A ..... GENERAL NOTES
- 2B ..... SCOPE OF WORK AND DETAILS
- 2C ..... ESTIMATED ROADWAY QUANTITIES AND FOOTNOTES
- 2D ..... ESTIMATED BRIDGE QUANTITIES
- 2E ..... DETAILS (EXCESS DISPOSAL SITE @ METRO AIRPORT)

- \*\* 3 ..... PRESENT LAYOUT SHEET
- 3A ..... PROPOSED LAYOUT SHEET
- 3B ..... PROFILES (I-440 AND RAMP 'N-F')
- 3C ..... CONTOUR GRADING PLAN
- \*\* 4 ..... UTILITIES

BRIDGE, L & N RAILROAD OVER I-440 @ STA. 4.25+83.71

- 5 ..... LAYOUT DWG. NO. M-82-142
- 6-80 ..... DETAILS DWG. NO. M-82-143 THRU M-82-167

BRIDGE, L & N RAILROAD OVER RAMP 'N-F' @ STA. 6+17.04

- 31 ..... LAYOUT DWG. NO. M-82-124
- 32-40 ..... DETAILS DWG. NO. M-82-125 THRU M-82-141A

50-60 ..... ROADWAY CROSS SECTIONS

\*\* SHEET NO. 3 IS FILED IN FILE IN 210 UNDER PROJ. I-440-4(C)209 R.O.W.  
 SHEET NO. 4 IS FILED IN FILE IN 308 UNDER PROJ. I-440-4(33)211.

**DWG NO. CURRENT REVISION DATE SHEET NAME**

- ROADWAY DESIGN STANDARDS**
- RD-A ..... 9-18-79 ..... STANDARD ABBREVIATIONS.
  - RD-L-1 ..... 8-15-76 ..... STANDARD LEGEND.
  - RD-L-2 ..... 9-18-79 ..... STANDARD LEGEND.
  - RD-S-11 ..... 4-4-77 ..... ROADSIDE SLOPE DEVELOPMENT
  - RD-S-11A ..... 4-4-77 ..... ROADSIDE DITCH DETAILS FOR DESIGN AND CONSTRUCTION.
  - RD-SA-1 ..... 4-4-77 ..... SAFETY APPROACH TO UNDERPASSES - GRADING DESIGN.
  - RD-SE-1 ..... 11-2-76 ..... STANDARD SUPERELEVATION DETAILS.

- DRAINAGE - CULVERTS AND ENDWALLS**
- D-CE-1 ..... 1-1-76 ..... CONSTRUCTION DETAILS
  - D-PB-1 ..... 2-25-77 ..... CLASS "B" BEDDING AND CULVERT EXCAVATION.
  - D-PE-4 ..... ..... CONCRETE ENDWALLS, STRAIGHT "U" AND "L" TYPE.
  - D-P6-4 ..... 3-2-76 ..... FERROUS AND ALUMINUM CORRUGATED METAL PIPE.

- DRAINAGE - CATCH BASINS AND MANHOLES**
- D-CB-3 ..... 7-17-81 ..... NOS. 36 AND 37 CATCH BASIN AND GRATE.

- SAFETY APPURTENANCES AND FENCE**
- S-F-10 ..... 7-17-81 ..... STANDARD RIGHT-OF-WAY STOCK FENCE.
  - S-F-10b ..... 7-17-81 ..... STANDARD RIGHT-OF-WAY CHAIN LINK FENCE.

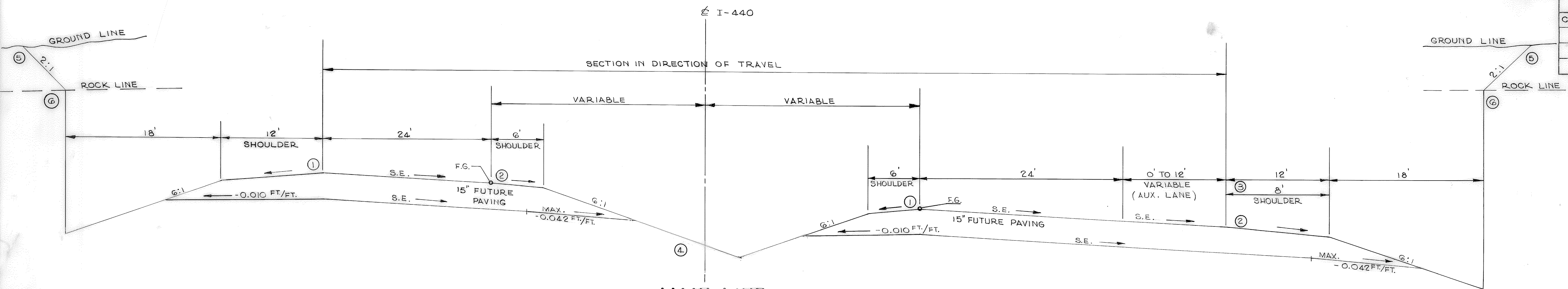
- EROSION CONTROL AND LANDSCAPING**
- EL-1-1 ..... 7-17-81 ..... TEMPORARY POLLUTION CONTROL STRUCTURES.
  - EL-2-2 ..... 7-17-81 ..... TYPICAL TEMPORARY POLLUTION CONTROL STRUCTURE LOCATIONS

STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION  
 BUREAU OF HIGHWAYS

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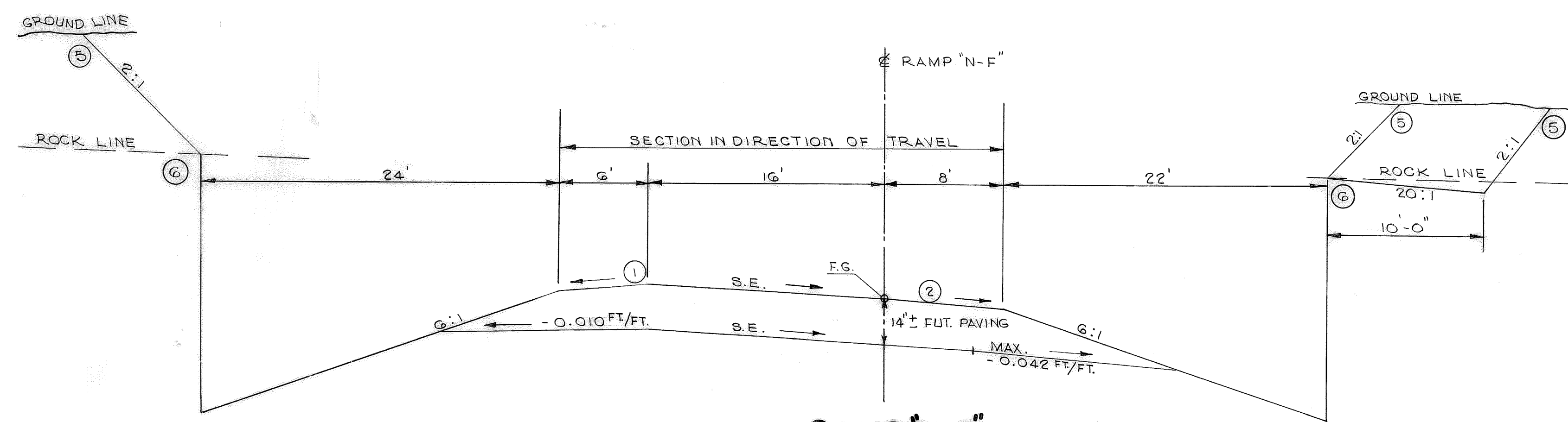
TELETYPE UNIT 432899

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	1981	I-440-4(45)212	2

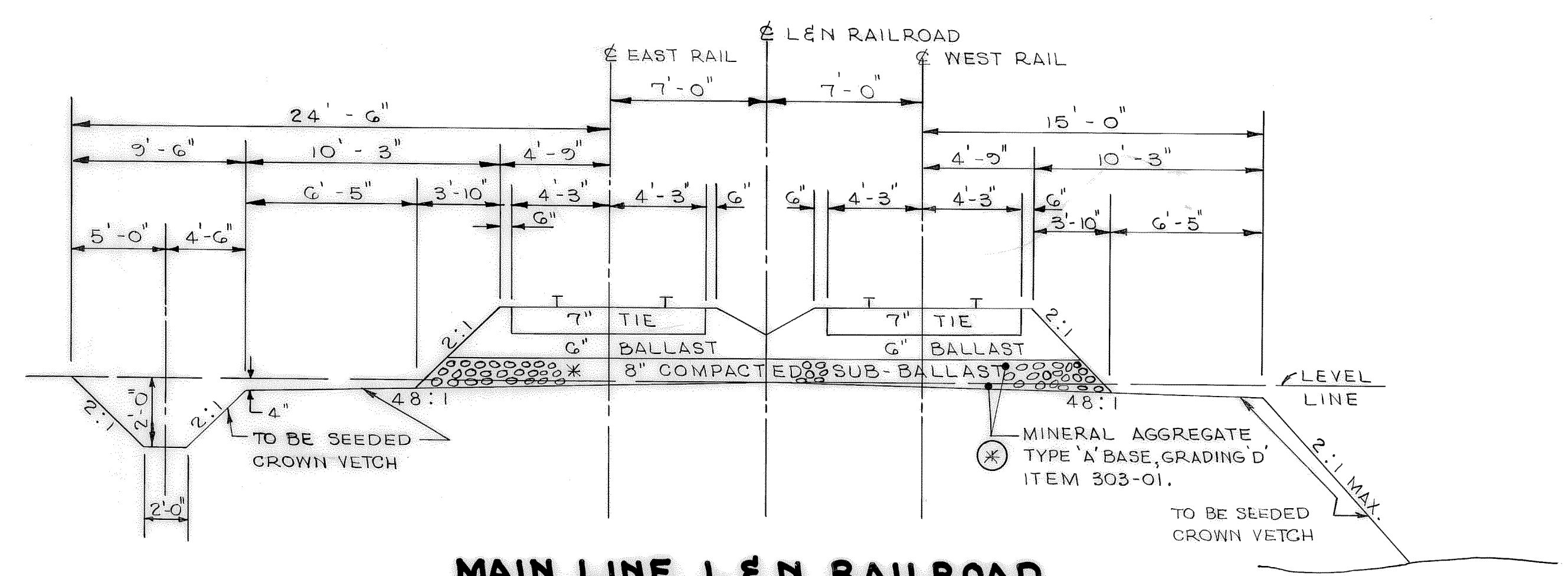


**MAIN LINE  
SUPERELEVATED SECTION**  
STA. 4+26+25 ± TO STA. 427+60 ±

- ① ROLL-OVER ALGEBRAIC DIFFERENCE IN RATES CROSS-SLOPES NOT TO EXCEED -0.07 FT/FT.
- ② SEE THIS SHEET FOR DETAIL OF LOW SHOULDER IN SUPERELEVATION.
- ③ SHOULDER WIDTH 8'-0" WHEN AUXILIARY LANE IS PRESENT. SEE PLANS FOR LOCATION OF AUXILIARY LANE.
- ④ AT LOCATIONS WHERE THE MEDIAN DITCH IS BELOW THE ROCK LINE, THE MEDIAN DITCH IS TO BE UNDER-CUT TO A DEPTH OF 12" ± AND BACK-FILLED WITH 9" ± OF COMMON EXC., AND 3" ± OF TOPSOIL AND SEEDED WITH CROWN VETCH.
- ⑤ ALL 2:1 SLOPES IN ROCK CUT ARE TO BE SEEDED CROWN VETCH.
- ⑥ ADD A 10' WIDE ROCK BENCH AT THE SOIL-ROCK CONTACT LINE, WHERE R.O.W. WIDTH PERMITS THIS TO BE DONE.



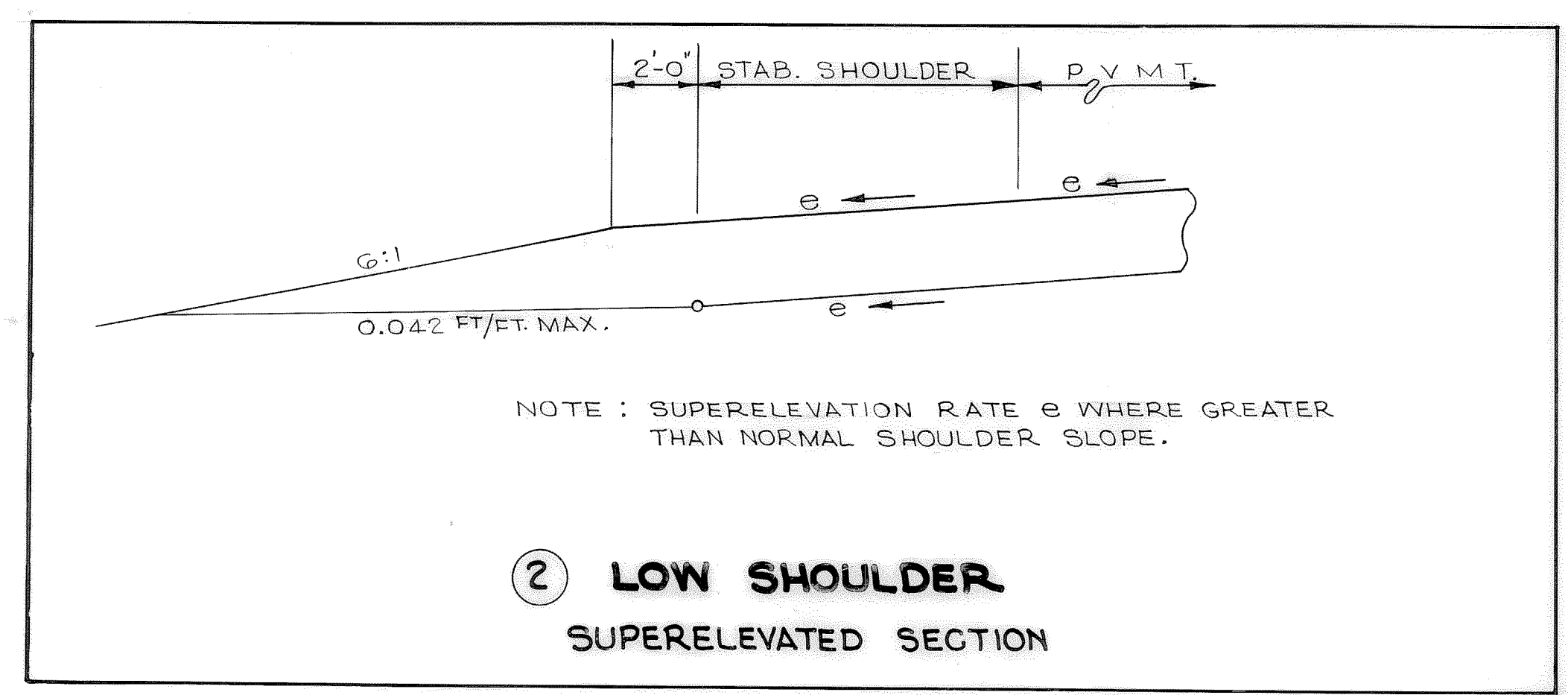
**RAMP "N-F"  
SUPERELEVATED SECTION**  
STA. 4+90 ± TO STA. 8+25 ±



**MAIN LINE L&N RAILROAD**  
STA. 385+50 TO STA. 397+35

\* THIS CONTRACTOR IS TO DO THE GRADING WORK AND INSTALL THE 8" COMPACTED SUB-BALLAST; THE L&N RAILROAD PERSONNEL WILL INSTALL THE 6" BALLAST AND DO THE FINAL TRACK WORK.

\* THE CONTRACTOR HAS THE OPTION OF USING THE EXISTING STONE ALONG THE RAILROAD IN LIEU OF ITEM 303-01. THE CONTRACTOR MUST CO-ORDINATE STOCK-PILING THE EXISTING STONE WITH THE ENGINEER AND THE L&N R.R.



② **LOW SHOULDER  
SUPERELEVATED SECTION**

NOTE: SUPERELEVATION RATE  $\epsilon$  WHERE GREATER THAN NORMAL SHOULDER SLOPE.

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
BUREAU OF HIGHWAYS

**TYPICAL SECTIONS**



# GENERAL NOTES

## MISCELLANEOUS

- ① THE THEORETICAL GRADE LINES SHOWN ON THE PLANS REPRESENTS THE FINISHED PAVEMENT SURFACE.
- ② SUPERELEVATIONS AND CROWNS ARE PLUS OR MINUS FROM THE THEORETICAL GRADE LINE .SEE STD. DWG. NO. RD-SE-1, ROADWAY CROSS-SECTIONS AND PROFILE SHEETS FOR DETAILS.
- ③ NO OPEN BURNING WILL BE ALLOWED ON THIS PROJECT.
- ④ NO SINKHOLES HAVE BEEN OBSERVED ON THIS PROJECT. THE POSSIBILITY DOES EXIST., THAT THERE MAY BE SOME ENCOUNTERED DURING THE GRADING OPERATIONS. IF THIS SITUATION OCCURS, THE DIVISION OF SOILS AND GEOLOGY WILL BE NOTIFIED FOR INVESTIGATION AND REMEDIAL RECOMMENDATIONS IF NEEDED. IF ANY OTHER SOILS OR GEOLOGICAL PROBLEMS ARE ENCOUNTERED DURING CONSTRUCTION THE DIVISION OF SOILS AND GEOLOGY WILL BE NOTIFIED.
- ⑤ LOCATION OF UTILITIES PUBLIC AND/OR PRIVATE ARE APPROXIMATE ONLY. THEIR EXACT LOCATION SHALL BE DETERMINED IN THE FIELD BY THE CONTRACTOR.
- ⑥ SEEDING (CROWN VETCH MIXTURE) WILL BE PLACED AT LOCATIONS SHOWN ON THE PLANS, ON ALL NEWLY GRADED CUT AND FILL SLOPES AS WORK PROGRESSES, AS DIRECTED BY THE ENGINEER. SEED (CROWN VETCH MIXTURE) ALL EARTH SLOPES AND OTHER SLOPES WHERE SEEDING IS REQUIRED TO PREVENT DAMAGE TO ADJACENT FACILITIES AND PROPERTY DUE TO EROSION.
- ⑦ WHERE SEED (CROWN VETCH MIXTURE) IS TO BE PLACED ON ROCK CUTS THE ROCK SHALL BE REMOVED 1'-0" BELOW THE GRADING LINE AND BACKFILLED WITH 9" OF EARTHEN MATERIAL AND 3" OF TOPSOIL PRIOR TO PLACING THE SEED. PAYMENT FOR THIS WORK WILL BE MADE UNDER ITEM NOS. 203-01, 203-04, AND 801-01.02.
- ⑧ DITCH QUANTITIES, IN GENERAL, ARE BASED UPON THE PAVING OF ALL EARTHEN DITCHES HAVING GRADES OF 3% OR MORE, AND THE SODDING OF THOSE HAVING GRADES LESS THAN 3%.
- ⑨ THE ENGINEER IN CHARGE OF PROJECT CONSTRUCTION MAY DECIDE THAT SOME DITCHES WITH GRADES 3% OR STEEPER WOULD FUNCTION WITHOUT EROSION IF SODDED OR THAT SOME DITCHES WITH GRADES FLATTER THAN 3% SHOULD BE PAVED. PAYMENT WILL BE ADJUSTED FOR THE INCREASE AND/OR DECREASE IN QUANTITIES AT THE PRICE BID.
- ⑩ FORMED OR SAWED CONTRACTION JOINTS (1/4" WIDE AND 1" DEEP) WILL BE REQUIRED AT A MAXIMUM OF 20' INTERVALS AND ONE-HALF (1/2) INCH TRANSVERSE PREMOULDED FIBER EXPANSION JOINTS WILL BE REQUIRED AT 60' INTERVALS IN ALL CONCRETE PAVED DITCHES, COST TO BE INCLUDED IN THE PRICE BID FOR THE PAY ITEM FOR PAVED DITCHES.
- ⑪ THE CONTRACTOR WILL BE REQUIRED TO SHAPE DITCHES TO THE SPECIFIED DESIGN, ALL COSTS TO BE INCLUDED IN THE PRICE BID FOR THE PAY FOR PAVED DITCHES.
- ⑫ THE ATTENTION OF THE CONTRACTOR IS DIRECTED TO THE FACT THAT HE WILL NOT HAVE EXCLUSIVE OCCUPANCY WITHIN AND ADJACENT TO THE LIMITS OF THIS PROJECT. SUBSEQUENT CONTRACTS FOR LANDSCAPING, NOISE WALLS, LIGHTING, PAVING ARE ANTICIPATED DURING THE WORK ON THIS CONTRACT. THIS CONTRACTOR SHALL NOT RESTRICT RIGHT OF ACCESS TO ANY SUBSEQUENT CONTRACTORS OR OTHERS TO WHOM THE STATE MAY GRANT THE RIGHT TO WORK WITHIN THE PROJECT AREA.
- ⑬ THE CONTRACTOR MUST EXERCISE CAUTION IN EXCAVATION AND BLASTING IN THE AREA OF THE PROPOSED BRIDGES, SO AS NOT DAMAGE OR INTERFERE WITH ANY WORK BEING DONE OR ALREADY DONE AT THE NOLENSVILLE ROAD BRIDGE.
- ⑭ THE CONTRACTOR, AFTER CONSULTING PROJECT ENGINEER, WILL BE REQUIRED TO NOTIFY OFFICIALS OF THE L. & N. RAILROAD PRIOR TO DOING ANY BLASTING IN THE VICINITY OF THE L. & N. RAILROAD TRACKS UNDER THIS PROJECT.
- ⑮ ENTRANCE AND EXIT FROM LOCAL STREETS TO PRIVATE PROPERTY MUST BE PROVIDED AT ALL TIMES. ALL COSTS FOR MAINTENANCE AND PROTECTION OF TRAFFIC IS TO BE INCLUDED IN OTHER ITEMS OF CONSTRUCTION.
- ⑯ THE CONTRACTOR MAY BE REQUIRED TO GRADE CERTAIN SOLID ROCK CUTS AS INDICATED ON THE CONTOURED GRADING PLAN OF THE L. & N. RAILROAD AREA. THESE AREAS WILL BE UNDERCUT TO A DEPTH OF 12" BELOW CONTOURS SHOWN; THEN BACKFILLED WITH 9" OF EARTHEN MATERIAL AND 3" OF TOPSOIL PRIOR TO PLACING THE SEEDING (CROWN VETCH). COST TO BE INCLUDED IN THE UNIT PRICE BID FOR ITEM NOS. 203-01, 203-04, AND 801-01.02.
- ⑰ ALL EXCESS DISPOSAL SITES SHALL BE APPROVED BY THE ENGINEER. THE ENGINEER WILL BE REQUIRED TO CONSULT THE REGIONAL CONSTRUCTION OFFICE, AND THE TRANSPORTATION ADMINISTRATOR - ENVIRONMENTAL PLANNING PRIOR TO APPROVING ANY DISPOSAL SITES. A POTENTIAL EXCESS SITE IS THE PROPOSED AIRPORT EXPANSION AT METRO AIRPORT (SEE SHEET NO. 2 E IF AIRPORT SITE IS USED.)

CONTINUED

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	1981	I-440-4(45)212	2 A

REV. 12-2-81: GENERAL NOTE # 27 REVISED.

- ⑱ BEFORE THE DEPARTMENT WILL APPROVE A SITE FOR DISPOSAL OF THE EXCESS MATERIAL, AT A PLACE OTHER THAN THE AIRPORT SITE, CONSIDERATION WILL BE GIVEN TO THE AESTHETICS OF THE AREA AFTER THE MATERIAL IS PLACED, THE LIKELIHOOD OF IMPAIRMENT OF DRAINAGE PATTERNS CAUSED BY PLACEMENT OF THE MATERIAL, AND THE WAY THE SITE WILL BE LEFT IN REGARD TO DEVELOPMENT POTENTIAL, ETC.. THE CONTRACTOR WILL BE REQUIRED TO GET AN APPROVED GRADING PERMIT FROM THE METROPOLITAN GOVERNMENT FOR ANY SITE IN DAVIDSON COUNTY AND THE APPROPRIATE PERMIT FROM APPROPRIATE LOCAL GOVERNMENT, IF ANY MATERIAL IS DISPOSED OF OUTSIDE DAVIDSON COUNTY.
- ⑲ THE CONTRACTOR WILL NOT BE ALLOWED TO HAUL EXCAVATED MATERIAL ACROSS OR ON NOLENSVILLE ROAD AT GRADE EXCEPT THAT MATERIAL BEING TRANSPORTED AS EXCESS MATERIAL. IF NOLENSVILLE ROAD IS USED AS A ROUTE FOR THE HAULING AWAY OF EXCESS MATERIAL, THE CONTRACTOR WILL NOT BE PERMITTED TO HAUL THIS MATERIAL THROUGH THE NOLENSVILLE ROAD AND THOMPSON LANE INTERSECTION DURING THE PEAK HOURS OF 7:00 AM. TO 9:00 AM. AND 3:30 P.M. TO 5:30 P.M.
- ⑳ THE ABATEMENT OF CONSTRUCTION NOISE IS A HIGH PRIORITY AREA ON THIS PROJECT, MEANING THAT THE CONTRACTOR WILL BE REQUIRED TO PERFORM HIS NORMAL CONSTRUCTION OPERATIONS BETWEEN THE HOURS OF 6:00 A.M. AND 7:00 P.M. AS DIRECTED BY THE ENGINEER. THIS INCLUDES BLASTING, DRILLING, OPERATION OF HEAVY CONSTRUCTION EQUIPMENT, ETC., AS FAR AS BLASTING IS CONCERNED THE CONTRACTOR WILL BE REQUIRED TO COMPLY WITH THE 1975 BLASTING LAW, WHICH IS ADMINISTERED BY THE TENNESSEE DEPARTMENT OF INSURANCE AND BANKING. THIS LAW REQUIRES CERTIFICATION OF THE PERSONNEL IN THE FIELD AND ESTABLISHES CRITERIA RELATING TO SIZE, TIMING, AND MAGNITUDE OF THE BLAST, AND THE DISTANCE FROM ANY NEARBY BUILDINGS OR IMPROVEMENTS. SHOULD ANY DAMAGES OCCUR DUE TO BLASTING, THE CONTRACTOR WILL BE HELD RESPONSIBLE.
- ㉑ THE COST OF REMOVING EXISTING PIPES (20" PIPE AT ORIEL AVE. & L. & N. R.R., 36" C.M. PIPE ALONG I-440, AND ANY OTHER PIPES ENCOUNTERED DURING THE CONSTRUCTION OF THIS PROJECT) WILL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM NO. 203-01, ROAD AND DRAINAGE EXCAVATION (UNCLASSIFIED).
- ㉒ UPON COMPLETION OF THE SUB-BALLAST WORK ON THE L. & N. RAILROAD, THE CONTRACTOR, AFTER CONSULTING WITH THE PROJECT ENGINEER, WILL BE REQUIRED TO NOTIFY THE L. & N. RAILROAD OFFICIALS TO CONDUCT A FINAL INSPECTION AND ACCEPTANCE OF THE SUB-BALLAST WORK.
- ㉓ THE ROCK MATERIAL LOCATED UNDER THE L. & N. RAILROAD. RUNAROUND, AND AT RAMP 'N-F' WILL BE SHOT BY OTHERS ON PROJECT NO. I-440-4(33)211. WHEN IT IS TIME TO REMOVE THIS MATERIAL AND IF IT IS FOUND THAT THE ROCK WAS NOT COMPLETELY SHOT TO CONTOUR GRADE AS SPECIFIED, THE CONTRACTOR FOR THIS PROJECT (I-440-4(45)212) WILL BE REQUIRED TO RE-SHOOT THE ROCK AS NECESSARY TO FINISH THIS PROJECT. THE COST OF THIS WORK TO BE INCLUDED IN THE UNIT PRICE BID FOR ITEM NO. 203-01 (ROAD AND DRAINAGE EXCAVATION UNCLASSIFIED).
- ㉔ EXISTING ROADWAYS WITHIN THE RIGHT-OF-WAY, WHICH ARE TO BE ABANDONED SHALL BE SCARIFIED, RE-GRADED, AND SEEDED (CROWN VETCH MIXTURE). THIS WORK TO BE AS DIRECTED BY THE ENGINEER, COST TO BE INCLUDED IN ITEM NO. 801-01.02.
- ㉕ THE CONTRACTOR ON THIS PROJECT WILL NOT RECIEVE DIRECT PAYMENT FOR TEMPORARY TRAFFIC CONTROL ITEMS; HOWEVER IF THE ENGINEER DETERMINES THAT SOME TEMPORARY TRAFFIC CONTROL ITEMS ARE REQUIRED, THEY WILL BE FURNISHED BY THIS CONTRACTOR. COST TO BE INCLUDED IN OTHER ITEMS OF CONSTRUCTION.
- ㉖ THE COST OF EXCAVATION FOR PIPE CULVERTS AND CATCHBASINS WILL BE INCLUDED IN THEIR RESPECTIVE PAY ITEMS, AND WILL NOT BE PAID FOR DIRECTLY.
- ㉗ FENCING SHALL BE ACCORDING TO STANDARD DRAWING S-F-10B EXCEPT AS NOTED HERE: VINYL COATED FABRIC IN ACCORDANCE WITH AASHTO M-181-77, TYPE IX VINYL COATED FABRIC. SIZE OF COATED WIRE SHALL BE 9 GAGE. CHAIN LINK FENCE FABRIC SHALL BE COATED WITH .30 OZ. PER FOOT OF GALVANIZING UNDER VINYL. COLOR OF FABRIC SHALL BE GREEN AND A COLOR SAMPLE SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL. POST, BRACES, RAIL AND ALL OTHER FRAMEWORK FOR FENCES SHALL COMPLY WITH STANDARD DRAWING S-F-10B. ALL FENCE ON THIS PROJECT WILL BE WITHOUT BARBED WIRE.
- ㉘ LOCATION OF CHAIN LINK FENCE WILL BE ONE FOOT INSIDE THE RIGHT-OF-WAY LINE EXCEPT WHERE OTHERWISE SHOWN ON THE PLANS. FENCE SHALL BE TURNED IN AT THE BRIDGE ABUTMENT ON RAMP 'N-F'. THE CONTRACTOR SHALL BE REQUIRED TO ERECT CERTAIN SECTIONS OF FENCING AT ANY TIME AS DIRECTED BY THE ENGINEER.
- ㉙ THE CONTRACTOR WILL BE REQUIRED TO USE EXTREME CAUTION IN WORKING AROUND EXISTING UTILITIES ALONG THE L. & N. RAILROAD, AND EXISTING STORM SEWER SYSTEM ALONG I-440 (CONSTRUCTED UNDER PROJECT I-440-4(33)211). IN THE EVENT ANY DAMAGE OCCURS TO THESE ITEMS DUE TO THE CONSTRUCTION OF THIS PROJECT THE CONTRACTOR WILL BE RESPONSIBLE FOR REPAIRING SAID DAMAGE.
- ㉚ THE EMBANKMENT MATERIAL FOR THE L. & N. RAILROAD IS TO BE, FOR THE MOST PART, ROCK FILL. THIS ROCK MATERIAL IS TO BE CAPPED WITH 9" OF EARTHEN MATERIAL AND 3" OF TOPSOIL AND SEEDING CROWN VETCH MIXTURE. PAYMENT FOR THIS WORK WILL BE MADE UNDER ITEM NOS. 203-01, 203-04, 203-07, AND 801-01.02.

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
BUREAU OF HIGHWAYS

GENERAL  
NOTES



**SCOPE - OF - WORK**

THE GRADING OF I-440 'PARKWAY', RAMP 'N-F', AND THE L & N RAILROAD TO LINES AND GRADES AS INDICATED ON THE TYPICAL CROSS-SECTIONS, PLAN AND PROFILE SHEETS, CONTOUR GRADING PLAN OR AS DIRECTED BY THE ENGINEER. SPECIAL ATTENTION SHOULD BE DIRECTED TO THE FACT THAT THIS CONTRACTOR WILL CONSTRUCT THE SUB-GRADE AND SUB-BALLAST ON THE L & N RAILROAD.

THE CONSTRUCTION OF 2 (TWO) BRIDGES:

- ① BRIDGE AND APPROACHES ON L & N RAILROAD OVER I-440
- ② BRIDGE AND APPROACHES ON L & N RAILROAD OVER NOLENSVILLE ROAD INTERCHANGE RAMP "N-F"

THE CONSTRUCTION OF THE SUB-BALLAST (8") ON THE L & N RAILROAD MAINLINE AS INDICATED ON THE TYPICAL SECTION, CROSS SECTIONS, OR AS DIRECTED BY THE ENGINEER (THE L & N RAILROAD PERSONNEL WILL INSTALL THE 6" BALLAST, 7" TIES, AND RAILS).

THE REMOVAL OF THE L & N RAILROAD RUNAROUND AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER.

FOLLOWING IS A SUGGESTED SEQUENCE OF RUNAROUND REMOVAL:

- ① L & N RAILROAD PERSONNEL WILL BE NOTIFIED TO REMOVE THE BALLAST, TIES AND RAILS. (THIS MATERIAL TO BECOME PROPERTY OF THE L & N RAILROAD).
- ② REMOVE THE EMBANKMENT ON THE RUN-AROUND TO LINES AND GRADES AS INDICATED ON THE CONTOUR GRADING PLAN, CROSS-SECTIONS OR AS DIRECTED BY THE ENGINEER.
- ③ REMOVE THE 27'-2" X 19'-1" CORRUGATED STEEL MULTI-PLATE STRUCTURE UNDER THE RUNAROUND, COST TO BE PAID FOR UNDER ITEM NO. 920-01.92. STRUCTURE TO BECOME PROPERTY OF THE STATE.
- ④ REMOVE THE 36" C.M. LEFT OF THE W.B.L. BEGINNING AT STA. 426+75± (I-440).

ALL STORM SEWER STRUCTURES AND PIPES, SEEDING (CROWN VETCH), SODDING, DITCH PAVING, TEMPORARY POLLUTION ITEMS, FENCE ADJUSTMENT, STONE MASONRY WALLS AND OTHER MISCELLANEOUS ITEMS FOR CONSTRUCTION OF THIS PROJECT.

**SEQUENCE OF WORK**

FOLLOWING IS A SUGGESTED SEQUENCE OF CONSTRUCTION FOR THIS PROJECT. THE CONTRACTOR MAY ELECT TO SET A DIFFERENT SEQUENCE OF CONSTRUCTION, IF HE DOES, THEN HE IS TO SUBMIT HIS SEQUENCE OF CONSTRUCTION TO THE PROJECT ENGINEER FOR APPROVAL.

- ① THE CONTRACTOR WILL BE REQUIRED TO CONSTRUCT THE EMBANKMENTS FOR THE L & N RAILROAD ON THE MAINLINE AND AT THE BRIDGE APPROACHES (I-440 AND RAMP 'N-F'). THE MATERIAL FOR THIS EMBANKMENT WILL BE OBTAINED FROM THE EXCAVATION ALONG RAMP 'N-F' FROM STA. 6+50± TO STA. 8+25±.
- ② AFTER THE BRIDGE AND APPROACH WORK IS COMPLETED ON THE MAINLINE OF THE L & N RAILROAD, AND THE TRAIN TRAFFIC HAS BEEN ROUTED BACK ON THE RAILROAD MAINLINE; THE CONTRACTOR WILL BE REQUIRED TO REMOVE THE L & N RAILROAD RUNAROUND (SEE SCOPE-OF-WORK FOR SEQUENCE OF RUNAROUND REMOVAL). THE MAJORITY OF THIS MATERIAL WILL BE EXCESS. SEE GENERAL NOTES AND SHEET NO. 2 E FOR DETAILS ON DISPOSAL SITES.

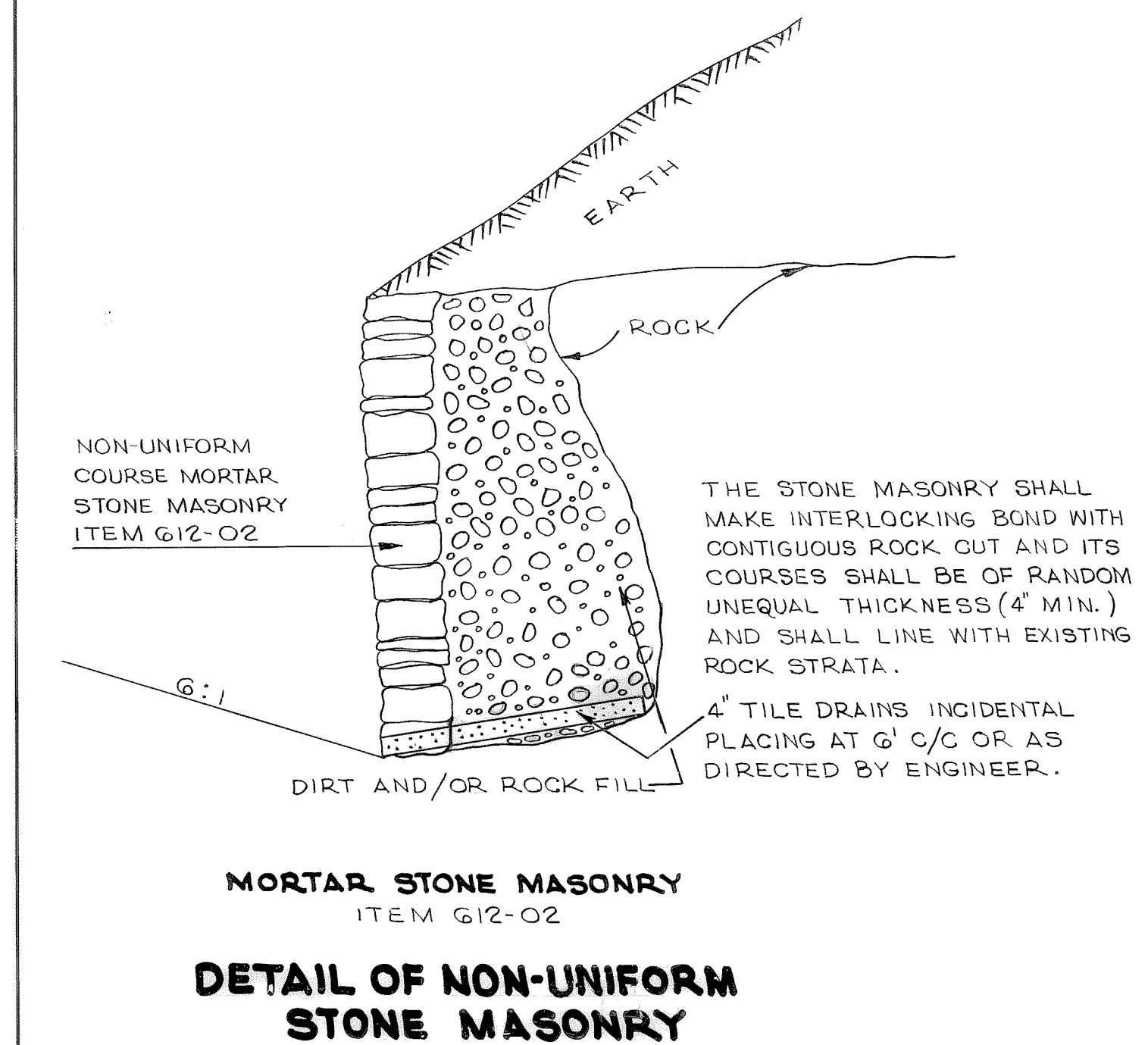
**RAILROAD ASSISTANT CHIEF ENGINEER**

MR. G. T. BONNER  
L & N RAILROAD COMPANY  
P. O. BOX 32200  
LOUISVILLE, KY. 40232

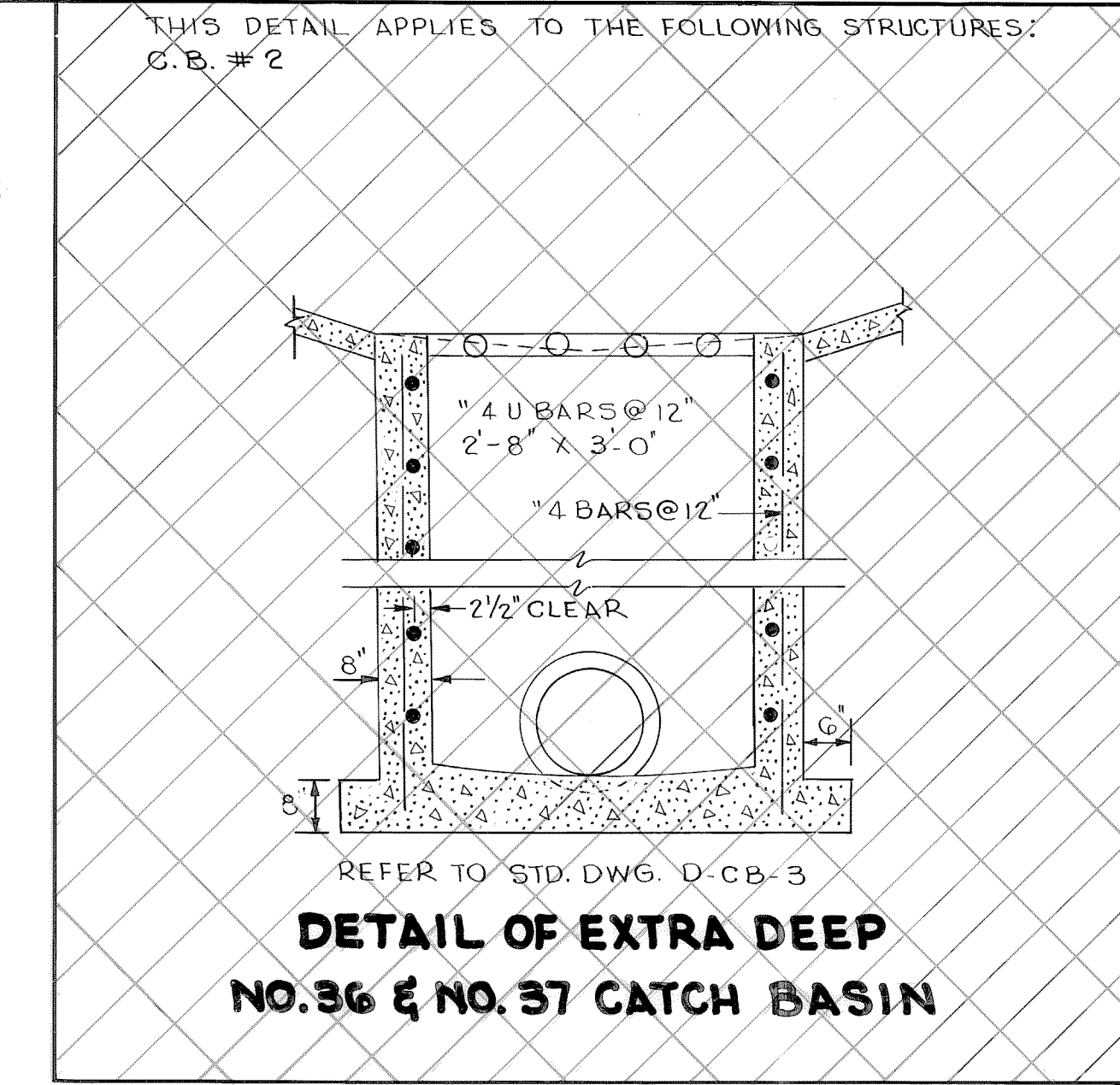
**RAILROAD SUPERINTENDENTS**

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NASHVILLE, TN., 37211

MR. E. W. BRATCHER  
L & N RAILROAD  
ATLANTA DIVISION  
P. O. BOX 1770  
ATLANTA GA 30301



**MORTAR STONE MASONRY  
ITEM G12-02**  
**DETAIL OF NON-UNIFORM  
STONE MASONRY**



REFER TO STD. DWG. D-CB-3  
**DETAIL OF EXTRA DEEP  
NO. 36 & NO. 37 CATCH BASIN**

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST	1981	I-440-4(45)212	2 B

**REV. 4-16-82:** DETAIL OF EXTRA DEEP CATCH BASIN DELETED FROM PLANS.

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
BUREAU OF HIGHWAYS

**SCOPE OF WORK  
AND  
DETAILS**

SCANNED BY...

TELETYPE UNIT



TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	1981	I-440-4(45)212	2C

REV. 4-16-82: QUANTITIES  
 REVISED ON ITEM NOS. 607-03.04,  
 607-37.02, 611-03.01 & 611-03.02.

### ESTIMATED ROADWAY QUANTITIES

ITEM NO.	DESCRIPTION	TOTAL	UNIT
109-10.01	TRAINEE	2,000	HOUR
203-01	ROAD AND DRAINAGE EXCAVATION (UNCLASSIFIED)	47,304	CU. YD.
203-01II	PRE SPLITTING OF ROCK EXCAVATION	1200	SQ. YD.
203-04	PLACING AND SPREADING TOPSOIL	778	CU. YD.
② 203-06	WATER	51	M. G.
203-07	FURNISHING AND SPREADING TOPSOIL	724	CU. YD.
204-07	BEDDING MATERIAL (PIPE) CLASS (B)	17	CU. YD.
209-01	TEMPORARY BERM	2900	LIN. FT.
209-02	TEMPORARY SLOPE DRAIN	220	LIN. FT.
209-03	CHECK DAMS	250	SQ. FT.
③ 209-05	SEDIMENT REMOVAL	650	CU. YD.
209-06	BALED HAY OR STRAW EROSION CHECKS	400	BALE
209-07	DUMPED ROCK	125	CU. YD.
④ 303-01	MINERAL AGGREGATE, TYPE "A" BASE, GRADING "D"	5,418	TON
303-06	CALCIUM CHLORIDE	325	BAG
607-03.04	18" CONCRETE PIPE CULVERT (CLASS V)	50	LIN. FT.
⑪ 607-3702	18" CORRUGATED METAL PIPE CULVERT	25	LIN. FT.
611-03.01	CLASS "A" CONCRETE (CATCH BASIN)	3	CU. YD.
611-03.02	STEEL BAR REINFORCING (CATCH BASIN)	25	LB.
611-07.01	CLASS "A" CONCRETE (PIPE ENDWALLS)	1	CU. YD.
611-07.02	STEEL BAR REINFORCING (PIPE ENDWALLS)	20	LB.
611-07.04	STRUCTURAL STEEL (CATCH BASIN)	144	LB.
③ 612-02	NONUNIFORM - COURSE STONE MASONRY	80	CU. YD.
③ 703-01	CEMENT CONCRETE DITCH PAVING	5	CU. YD.
⑩ 707-02	END AND CORNER POST ASSEMBLIES (CHAIN-LINK) FENCE - 6-FOOT	4	EACH
707-02.20	CHAIN-LINK FENCE (6-FOOT) (VINYL COATED)	130	LIN. FT.
717-01	MOBILIZATION	1	LUMP SUM
801-01.02	CROWN VETCH MIXTURE (WITH MULCH)	102	UNIT
⑥ 801-03	WATER (SEEDING AND SODDING)	15	M. - GAL.
801-07	SEED (SUPPLEMENTAL APPLICATION)	23	L. B.
⑦ 801-08	FERTILIZER (SUPPLEMENTAL APPLICATION)	1	TON
803-01	SODDING (NEW SOD)	750	SQ. YD.
⑨ 920-01.92	REMOVAL OF MULTI-PLATE STEEL STRUCTURE	1	LUMP SUM

— FOOTNOTES —

- ② INCLUDES 40' M' GAL. FOR EMBANKMENT AND 2' M' GAL. FOR GRANULAR BACKFILL.
- ③ QUANTITY MAY BE INCREASED, DECREASED, OR ELIMINATED AS DIRECTED BY THE ENGINEER.
- ④ INCLUDES 1,226 TONS TO BE USED FOR SUB-BALLAST ON THE L & N RAILROAD (THE CONTRACTOR MAY ELECT TO UTILIZE THE EXISTING SUB-BALLAST MATERIAL IN LIEU OF 303-01), INCLUDES 3,432 TONS FOR BACKFILLING BRIDGES.
- ⑥ INCLUDES 11' M' GAL. FOR 801-01.02, 8' M' GAL. FOR SODDING.
- ⑦ TO BE USED WITH ITEM 801-01.02.
- ⑨ INCLUDES TOTAL COST OF STRUCTURE REMOVAL (STRUCTURE UNDER L. & N. RAILROAD RUNAROUND) AND DELIVERY TO T. D.O.T. MAINTENANCE YARD ON CENTENNIAL BLVD. FOR STORAGE. SEE SPECIAL PROVISION NO. 202 A.
- ⑩ TO BE USED TO TIE INTO BRIDGE ENDS ALONG RAMP "N-F". ALL FENCE AND POST TO BE VINYL COATED GREEN, SEE GENERAL NOTE (27).
- ⑪ THE PIPE ENDS AT JOINTS SHALL BE REFORMED TO AN ANNULAR CORRUGATION, FOR A MINIMUM OF TWO CORRUGATIONS, AND A CORRUGATED BAND 12" WIDE SHALL BE USED TO MESH WITH THE CORRUGATIONS OF THE PIPE ENDS.

STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION  
 BUREAU OF HIGHWAYS

**ESTIMATED  
 ROADWAY  
 QUANTITIES**



REV. 6-22-83 HMB BRIDGE DRAWINGS FROM M-82 TO M-94.

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	1981	I-440-4(45)212	2D

BRIDGE NO. 1	BRIDGE NO. 2
STA. 425+83.71 I-440 SIMPLE STEEL WELDED PLATE GIRDER WITH COMPOSITE CONC. DECK SLAB OVER I-440 1- SPAN @ 106'-3" 1- SPAN @ 117'-9" LENGTH - 224'-0" 36'-0" WIDTH WITH 2'-6" WALKWAYS AND METAL RAILING SKEW 53° 34' 03" LAYOUT DWG. NO. M-94-142	STA. G+17.04 RAMP "N-F" SIMPLE STEEL WELDED PLATE GIRDER WITH COMPOSITE CONC. DECK SLAB OVER RAMP "N-F" 1- SPAN @ 86'-0" LENGTH 86'-0" VARIABLE WIDTH WITH 2'-6" WALKWAYS AND METAL RAILING SKEW 87° 54' 03" LAYOUT DWG. NO. M-94-124

**ESTIMATED BRIDGE QUANTITIES**

ITEM NO.	DESCRIPTION	QUANTITY	QUANTITY	TOTAL QUANTITIES	UNIT
202-11	CONSTRUCTION AND REMOVAL OF TEMPORARY STRUCTURES		1	1	LUMP SUM
204-02.01	DRY EXCAVATION (BRIDGES)	1,340	2,185	3,525	CU. YD.
204-04.01	ROCK EXCAVATION (BRIDGES)	645	318	963	CU. YD.
204-05	ROCK DRILLING (BRIDGES)	48	48	96	LIN. FT.
602-14.01	STEEL STRUCTURES (ERECTION)(STA. 425+83.71)	1		1	LUMP SUM
602-14.02	STEEL STRUCTURES (ERECTION)(STA. G+17.04)		1	1	LUMP SUM
602-18.01	STEEL STRUCTURES (FURNISHING DOMESTIC)(STA. 425+83.71)	1		1	LUMP SUM
602-18.02	STEEL STRUCTURES (FURNISHING DOMESTIC)(STA. G+17.04)		1	1	LUMP SUM
602-46.01	STEEL STRUCTURES (FURNISHING FOREIGN)(STA. 425+83.71)	1		1	LUMP SUM
602-46.02	STEEL STRUCTURES (FURNISHING FOREIGN)(STA. G+17.04)		1	1	LUMP SUM
604-03.01	CLASS "A" CONCRETE (BRIDGES)	1,341.6	975.3	2,316.9	CU. YD.
604-03.02	STEEL BAR REINFORCEMENT (BRIDGES)	156,456	111,476	267,932	LB.
605-03	WATERPROOFING (RAILROAD BRIDGE DECK)	757	291	1,048	SQ. YD.
605-04	WATERPROOFING CONSTRUCTION JOINTS (RAILROAD STRUCTURE)	160	193	353	SQ. YD.
605-05	DAMP PROOFING (RAILROAD STRUCTURE)	362	475	837	SQ. YD.
610-09.01	DRAINAGE SYSTEM RAILROAD (ABUTMENTS & RETAINING WALLS) (STA. 425+83.71)	1		1	LUMP SUM
610-09.02	DRAINAGE SYSTEM RAILROAD (ABUTMENTS & RETAINING WALLS) (STA. G+17.04)		1	1	LUMP SUM
610-10.01	DRAINAGE SYSTEM RAILROAD (BRIDGE DECK)(STA. 425+83.71)	1		1	LUMP SUM
610-10.02	DRAINAGE SYSTEM RAILROAD (BRIDGE DECK)(STA. G+17.04)		1	1	LUMP SUM
620-01	ALUMINUM RAILING	580.9	327.1	908	LIN. FT.

OPTIONAL  
OPTIONAL

**FOOTNOTES:**

BRIDGE NO. 1..... SEE SHEET NO. 6, DWG. NO. M-94-143  
BRIDGE NO. 2..... SEE SHEET NO. 32, DWG. NO. M-94-125

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
BUREAU OF HIGHWAYS

**BRIDGE  
QUANTITIES**

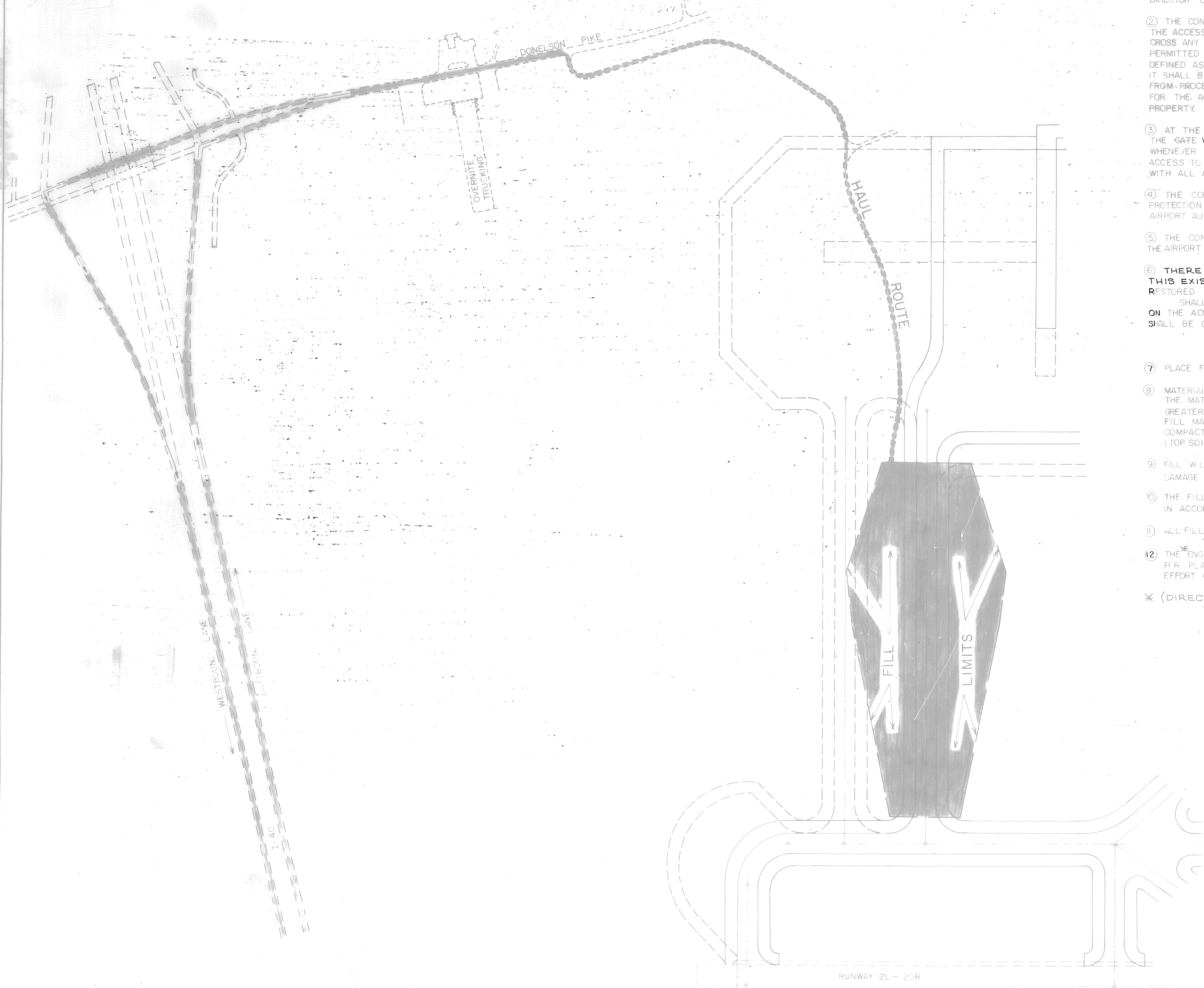


SCANNED  
FEB 11 2002  
BY TRC

TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	1981	I-440-4(45)212	2 E

NOTES

- ① THE CONTRACTOR MAY BE PERMITTED TO PLACE EXCESS MATERIAL ON A SITE AT METROPOLITAN NASHVILLE AIRPORT. THE LOCATION MAP INDICATES THE SITE FOR THE PLACEMENT OF THE EXCESS MATERIAL. THE CONTRACTOR WILL BE REQUIRED TO ADHERE TO THE CONDITIONS LISTED BELOW. FOR ENTRY TO INSPECT THE SITE, PLEASE CONTACT THE DIRECTOR OF PLANNING AND ENGINEERING, RICHARD MORGAN, 367-3003.
  - ② THE CONTRACTOR WILL BE PERMITTED ACCESS TO THE SITE FROM DONELSON PIKE AND ALONG THE ACCESS ROAD SHOWN ON THE MAP. THE CONTRACTOR WILL NOT BE PERMITTED TO CROSS ANY RUNWAY OR TAXIWAY ON THE AIRPORT PROPERTY. THE CONTRACTOR WILL NOT BE PERMITTED TO ENTER ANY AIRCRAFT OPERATIONS AREA. AIRCRAFT OPERATIONS AREA IS DEFINED AS ANY AREA WITHIN 500 FEET OF A RUNWAY, TAXIWAY, OR AIRCRAFT PARKING AREA. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PREVENT PERSONNEL OR EQUIPMENT FROM PROCEEDING INTO THESE IDENTIFIED AREAS. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE ACTIONS OF HIS EMPLOYEES OR SUB-CONTRACTORS WHILE THEY ARE ON AIRPORT PROPERTY.
  - ③ AT THE ENTRANCE TO THE AIRPORT, FROM DONELSON PIKE, THE CONTRACTOR SHALL MAN THE GATE WHICH SHALL BE LOCKED AT ALL TIMES WHILE WORK IS NOT IN PROGRESS. WHENEVER THE GATE IS OPEN, THE CONTRACTOR SHALL FURNISH A GUARD WHO WILL LIMIT ACCESS TO AUTHORIZED PERSONNEL ONLY. THE CONTRACTOR WILL BE REQUIRED TO COMPLY WITH ALL AIRPORT SAFETY AND SECURITY REGULATIONS.
  - ④ THE CONTRACTOR WILL BE RESPONSIBLE FOR SECURITY OF HIS EQUIPMENT AND NO PROTECTION OF VANDALISM OR THEFT WILL BE PROVIDED BY THE METROPOLITAN NASHVILLE AIRPORT AUTHORITY.
  - ⑤ THE CONTRACTOR MAY BE REQUIRED TO WORK WITH ANY OTHER CONTRACTOR WORKING ON THE AIRPORT DURING THE TIME FILL MATERIAL IS BEING PLACED.
  - ⑥ THERE IS AN EXISTING ROAD TO THE SITE FOR DISPOSAL OF MATERIAL. IF THIS EXISTING ROAD IS DAMAGED BY THE CONTRACTORS OPERATIONS, IT IS TO BE RESTORED TO THE ORIGINAL CONDITION AFTER THE COMPLETION OF THE WORK. THE ROAD SHALL BE MAINTAINED TO PREVENT EXCESSIVE BUST AND EROSION. AT CERTAIN LOCATIONS ON THE ACCESS ROAD, IT MAY BE REQUIRED TO INSTALL TEMPORARY CULVERTS. THESE CULVERTS SHALL BE OF ADEQUATE SIZE TO HANDLE ANTICIPATED FLOW.
  - ⑦ PLACE FILL WITHIN BOUNDARIES OF THE AREA SHOWN ON THE LOCATION MAP.
  - ⑧ MATERIAL MUST BE SUITABLE FOR CONSTRUCTING A FILL TO CARRY HEAVY AIRCRAFT LOADS. THE MATERIAL SHOULD CONTAIN A MINIMUM OF 75% CRUSHED ROCK WITH NO ROCK GREATER THAN A DIMENSION (LENGTH, WIDTH, OR DEPTH) GREATER THAN 3 FEET. THE FILL MATERIAL SHOULD CONTAIN ENOUGH SMALL PARTICLES TO FILL VOIDS TO BE COMPACTED TO FORM A DENSE MASS. THE FILL SHALL NOT CONTAIN ANY ORGANIC MATTER (TOP SOIL, VEGETATION, ECT.) OR NORMAL CONSTRUCTION DEBRIS.
  - ⑨ FILL WILL BE PLACED OVER DRAINAGE STRUCTURE. CARE MUST BE TAKEN TO PREVENT DAMAGE TO DRAINAGE STRUCTURE.
  - ⑩ THE FILL MATERIAL SHALL BE PLACED IN UNIFORM LAYERS, DISTRIBUTED AND COMPACTED IN ACCORDANCE WITH STANDARD TDOT SPECIFICATIONS FOR CONSTRUCTION OF A DEEP FILL.
  - ⑪ ALL FILL SLOPES SHOULD BE CONSTRUCTED AT 3:1.
  - ⑫ THE ENGINEER SHALL HAVE THE RIGHT TO CHANGE THE LOCATION ON AIRPORT PROPERTY FOR PLACEMENT OF FILL; HOWEVER, ANY NEW LOCATION WILL NOT REQUIRE MORE EFFORT OF THE CONTRACTOR THAN THE LOCATION SHOWN ON THE LOCATION MAP.
- \*(DIRECTOR OF PLANNING AND ENGINEER, RICHARD MORGAN, 3673003.)



STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION BUREAU OF HIGHWAYS	
EXCESS	DISPOSAL
SITE	Q
METRO	AIRPORT

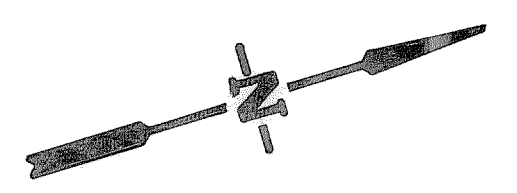






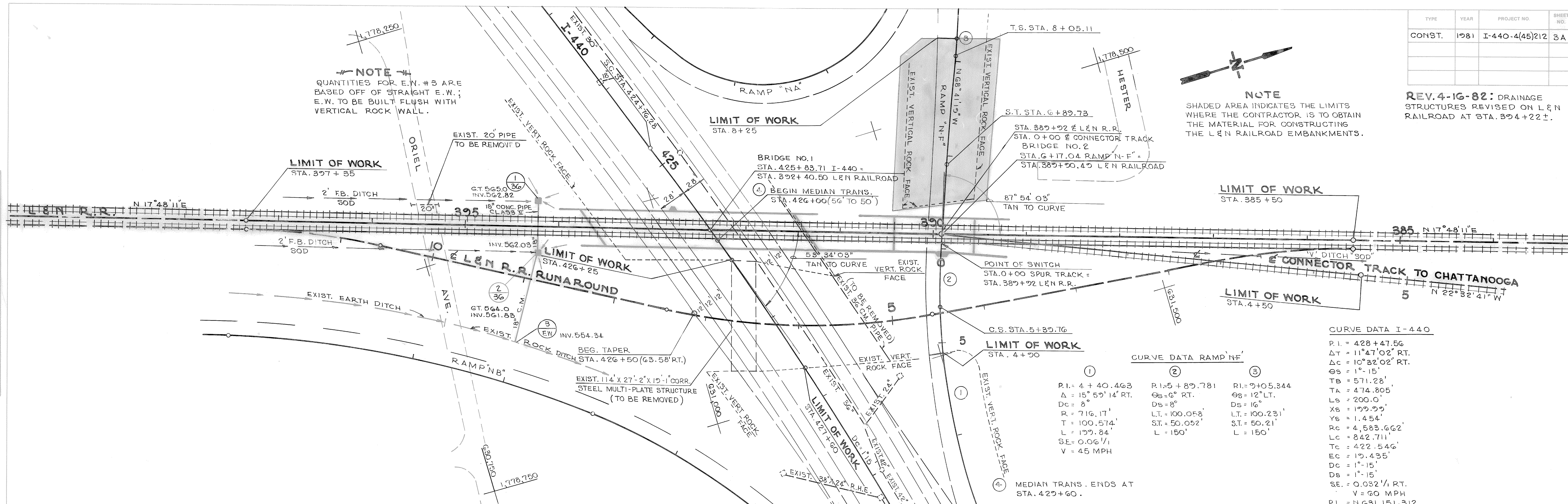
TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	1981	I-440-4(45)212	3A

REV. 4-16-82: DRAINAGE STRUCTURES REVISED ON L & N RAILROAD AT STA. 394+22±.



**NOTE**  
SHADED AREA INDICATES THE LIMITS WHERE THE CONTRACTOR IS TO OBTAIN THE MATERIAL FOR CONSTRUCTING THE L & N RAILROAD EMBANKMENTS.

**NOTE #1**  
QUANTITIES FOR E.W. #3 ARE BASED OFF OF STRAIGHT E.W.; E.W. TO BE BUILT FLUSH WITH VERTICAL ROCK WALL.



CURVE DATA I-440		
P.I. = 428+47.56		
ΔT = 11°47'02" RT.		
Δc = 10°32'02" RT.		
ΘS = 1°-15'		
TB = 571.28'		
TA = 474.805'		
Ls = 200.0'		
Ds = 8°		
Xs = 199.99'		
Ys = 1.454'		
Rc = 4,583.662'		
Lc = 842.711'		
Tc = 422.546'		
EC = 19.435'		
DC = 1°-15'		
Ds = 1°-15'		
SE = 0.032 1/1 RT.		
V = 60 MPH		
P.I. = N 631,151.312		
EI, 773,723.406		

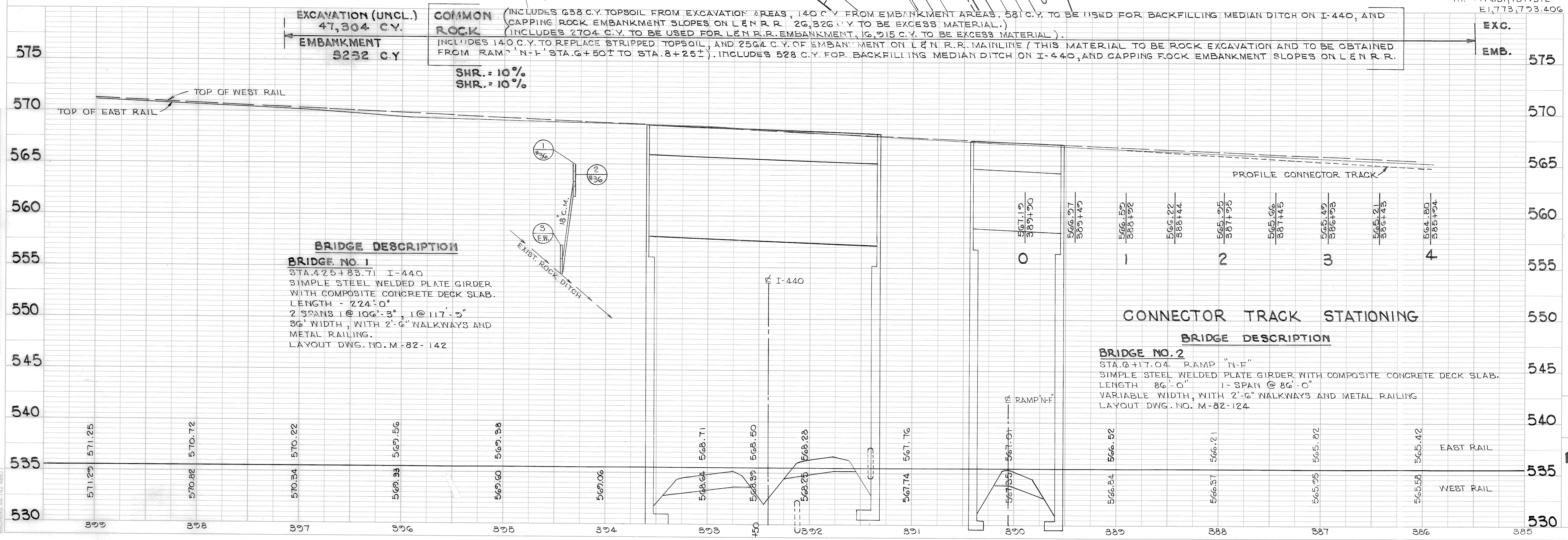
  

CURVE DATA RAMP 'N-F'		
P.I. = 4+40.463		
Δ = 15°59'14" RT.		
Dc = 8°		
R = 716.17'		
T = 100.574'		
L = 199.84'		
SE = 0.06 1/1		
V = 45 MPH		

①	②	③
P.I. = 5+89.781		
ΘS = 6° RT.		
Ds = 8°		
L.T. = 100.053'		
S.T. = 50.052'		
L = 150'		

**EXCAVATION (UNCL.)** 47,304 C.Y.  
**EMBANKMENT** 3232 C.Y.  
**COMMON ROCK** (INCLUDES 638 C.Y. TOPSOIL FROM EXCAVATION AREAS, 140 C.Y. FROM EMBANKMENT AREAS, 581 C.Y. TO BE USED FOR BACKFILLING MEDIAN DITCH ON I-440, AND CAPPING ROCK EMBANKMENT SLOPES ON L & N R.R. 26,326 C.Y. TO BE EXCESS MATERIAL.)  
 INCLUDES 140 C.Y. TO REPLACE STRIPPED TOPSOIL, AND 2564 C.Y. OF EMBANKMENT ON L & N R.R. MAINLINE (THIS MATERIAL TO BE ROCK EXCAVATION AND TO BE OBTAINED FROM RAMP 'N-F' STA. 6+50± TO STA. 8+25±). INCLUDES 528 C.Y. FOR BACKFILLING MEDIAN DITCH ON I-440, AND CAPPING ROCK EMBANKMENT SLOPES ON L & N R.R.  
 SHR. = 10%  
 SHR. = 10%



**BRIDGE DESCRIPTION**  
**BRIDGE NO. 1**  
 STA. 425+83.71 I-440  
 SIMPLE STEEL WELDED PLATE GIRDER WITH COMPOSITE CONCRETE DECK SLAB.  
 LENGTH - 224'-0"  
 2 SPANS 1 @ 106'-3", 1 @ 117'-3"  
 36" WIDTH, WITH 2'-6" WALKWAYS AND METAL RAILING.  
 LAYOUT DWG. NO. M-82-142

**CONNECTOR TRACK STATIONING**  
**BRIDGE DESCRIPTION**  
**BRIDGE NO. 2**  
 STA. 6+17.04 RAMP 'N-F'  
 SIMPLE STEEL WELDED PLATE GIRDER WITH COMPOSITE CONCRETE DECK SLAB.  
 LENGTH 86'-0"  
 1-SPAN @ 86'-0"  
 VARIABLE WIDTH, WITH 2'-6" WALKWAYS AND METAL RAILING.  
 LAYOUT DWG. NO. M-82-124

STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION  
 BUREAU OF HIGHWAYS  
**PROPOSED LAYOUT**  
 L & N RAILROAD  
 STA. 399+00 TO STA. 385+00



REV. 4-16-82: ELEVATION OF MULTI-PLATE STRUCTURE REVISED.

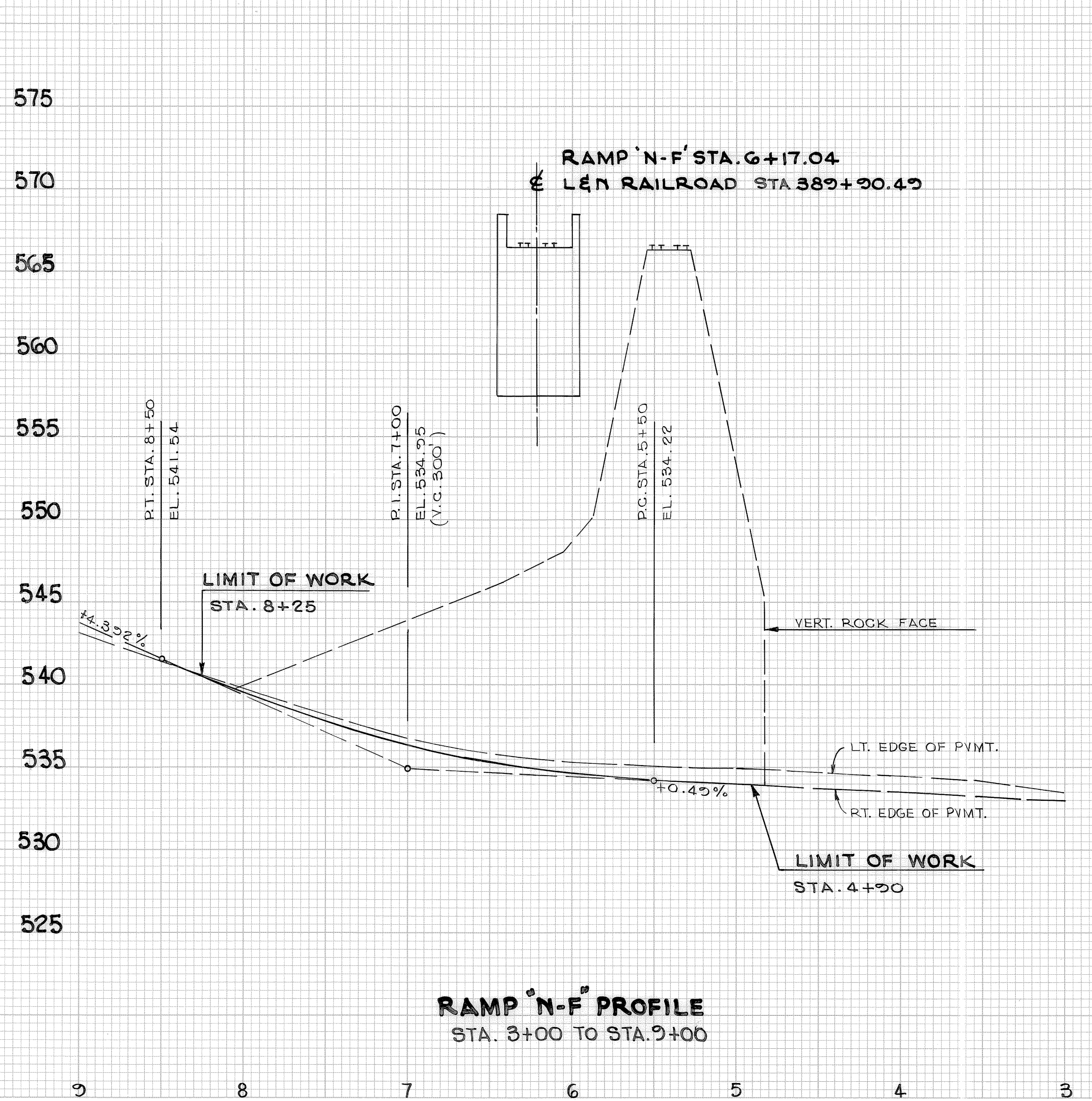
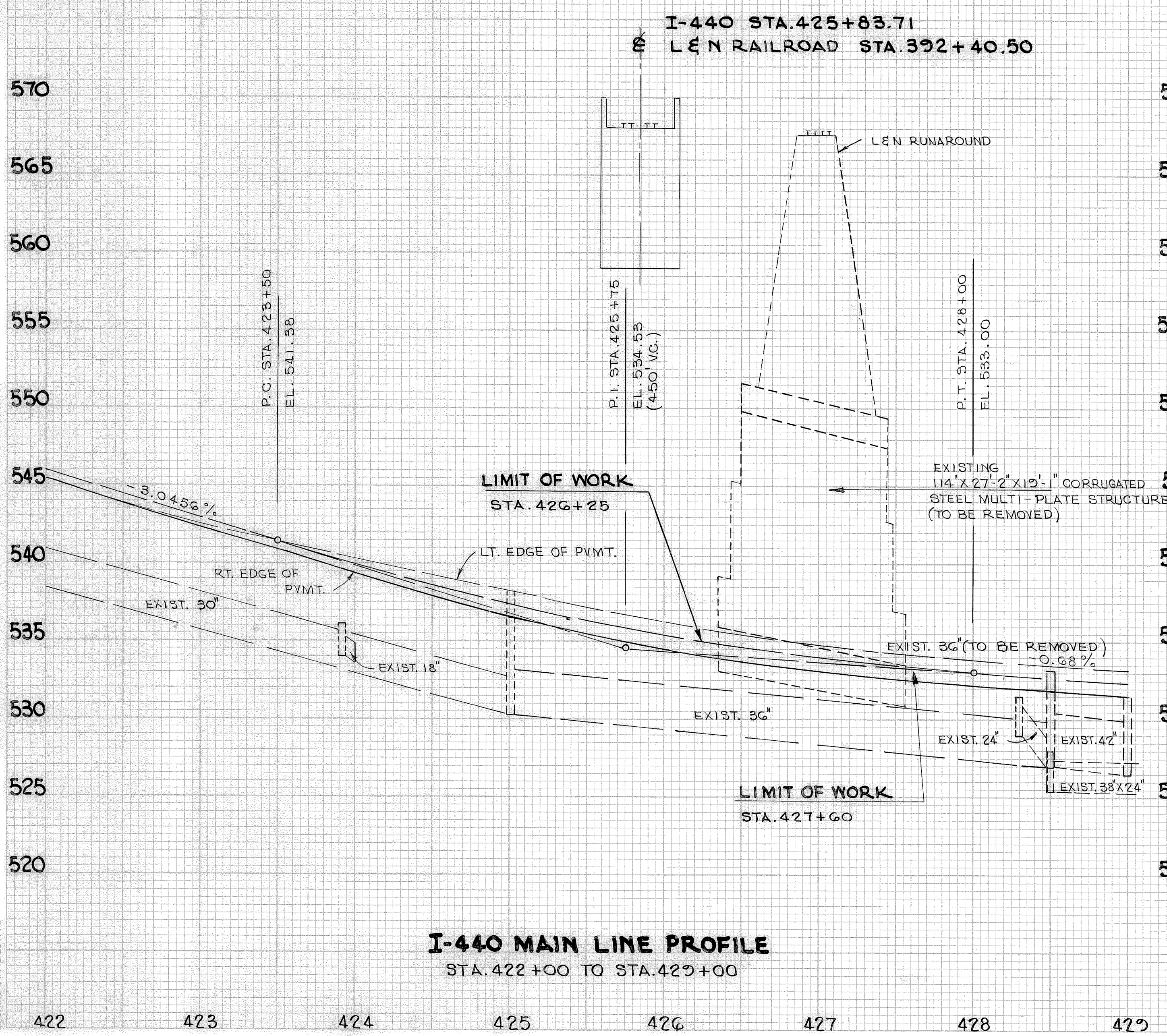
SCANNED

DATE	BY
DATE	BY

DATE	BY
DATE	BY

FINISHED GRADE EL.	RIGHT INSIDE EDGE OF PAVEMENT	LEFT INSIDE EDGE OF PAVEMENT
545.44	542.40	542.54
543.92	540.88	541.38
540.29	539.42	540.29
539.30	537.90	539.30
538.30	536.51	538.30
537.24	535.44	537.24
536.30	534.51	536.30
535.49	533.72	535.49
534.81	533.07	534.81
534.26	532.55	534.26
533.84	532.15	533.84
533.49	531.83	533.49
533.13	531.50	533.13

RIGHT EDGE OF PAVEMENT	LEFT EDGE OF PAVEMENT	FINISHED GRADE EL.
543.73	543.09	(9+00)
541.54	541.46	(8+50)
540.48	540.67	(8+25)
539.51	539.82	(8+00)
538.61	538.95	(7+75)
537.80	538.13	(7+50)
537.07	537.38	(7+25)
536.41	536.70	(7+00)
535.84	536.19	(6+75)
535.37	535.82	(6+50)
534.95	535.53	(6+25)
534.62	535.31	(6+00)
534.38	535.18	(5+75)
534.22	535.12	(5+50)
533.27	534.93	(5+00)
533.73	534.69	(4+50)
533.48	534.45	(4+00)
533.23	534.15	(3+50)
532.99	533.46	(3+00)



**I-440 MAIN LINE PROFILE**  
STA. 422+00 TO STA. 429+00

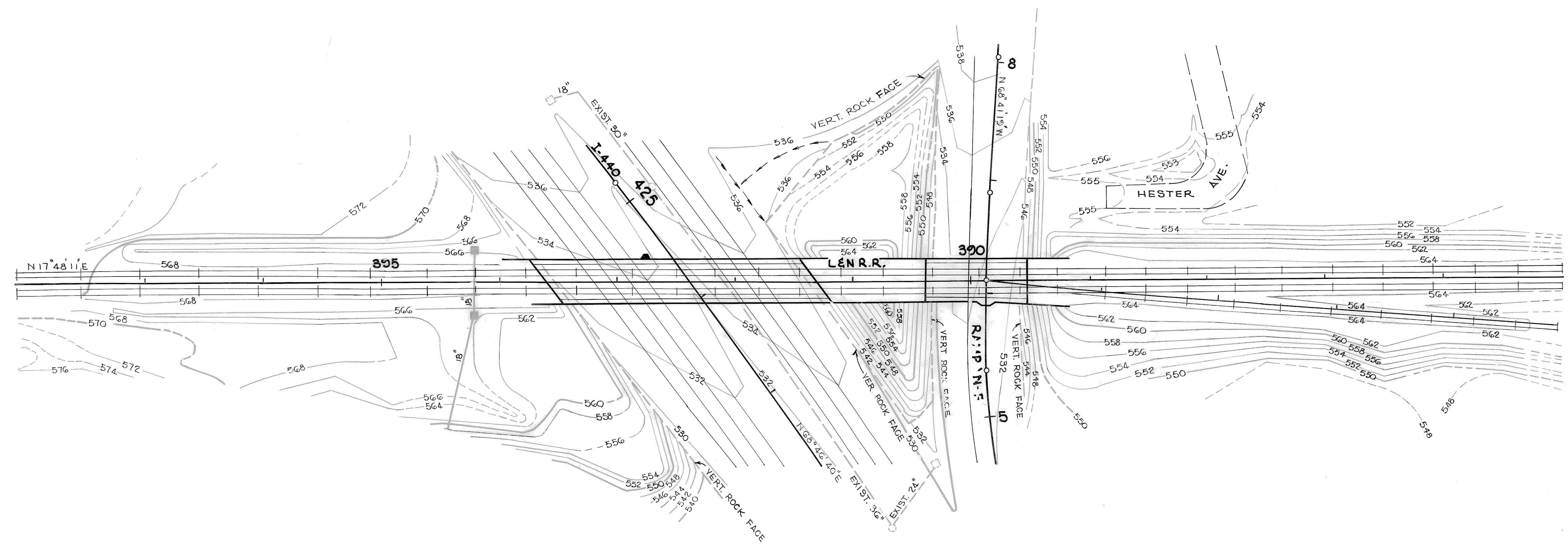
**RAMP 'N-F' PROFILE**  
STA. 3+00 TO STA. 9+00



TYPE	YEAR	PROJECT NO.	SHEET NO.
CONST.	1981	I-440-4(45)212	3C

REV. 4-16-82: DRAINAGE  
 STRUCTURES REVISED STA. 394+22'

SCA 405



STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION  
 BUREAU OF HIGHWAYS

**L & N CONTOUR  
 GRADING PLAN**

SCALE: 1" = 50'



NOTE: THE PROPOSED 12" WATER MAIN IN GRANDVIEW AVE. AND THE 2-12" WATER MAIN AT THE NOLENSVILLE RD. PUMPING STATION ARE TO BE IN SERVICE BEFORE THE 6" WATER MAIN IN NOLENSVILLE RD. IS KILLED.

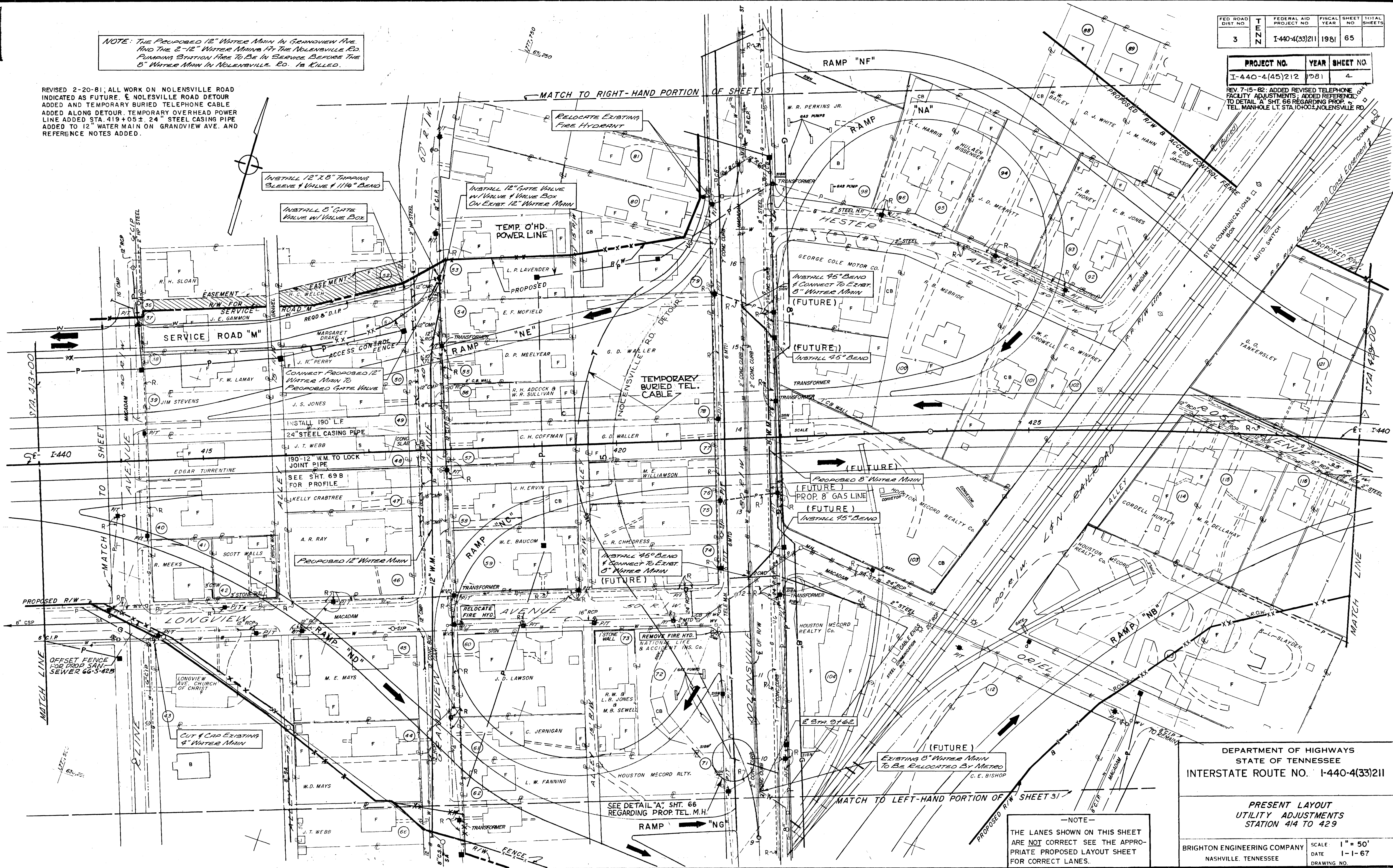
REVISED 2-20-61; ALL WORK ON NOLENSVILLE ROAD INDICATED AS FUTURE. NOLENSVILLE ROAD DETOUR ADDED AND TEMPORARY BURIED TELEPHONE CABLE ADDED ALONG DETOUR. TEMPORARY OVERHEAD POWER LINE ADDED STA. 419+05 ± 24" STEEL CASING PIPE ADDED TO 12" WATER MAIN ON GRANDVIEW AVE. AND REFERENCE NOTES ADDED.

FED. ROAD DIST. NO.	TENN.	FEDERAL AID PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
3		I-440-4(33)211	1961	65	

PROJECT NO.	YEAR	SHEET NO.
I-440-4(45)212	1961	4

REV. 7-15-62: ADDED REVISED TELEPHONE FACILITY ADJUSTMENTS; ADDED REFERENCE TO DETAIL "A" SHT. 66 REGARDING PROP. TEL. MANHOLE LT. STA. 100+02, NOLENSVILLE RD.



DEPARTMENT OF HIGHWAYS  
 STATE OF TENNESSEE  
 INTERSTATE ROUTE NO. I-440-4(33)211

PRESENT LAYOUT  
 UTILITY ADJUSTMENTS  
 STATION 414 TO 429

BRIGHTON ENGINEERING COMPANY  
 NASHVILLE, TENNESSEE

—NOTE—  
 THE LANES SHOWN ON THIS SHEET ARE NOT CORRECT SEE THE APPROPRIATE PROPOSED LAYOUT SHEET FOR CORRECT LANES.

SEE DETAIL "A" SHT. 66 REGARDING PROP. TEL. M.H.

MATCH TO LEFT-HAND PORTION OF SHEET 31

MATCH TO RIGHT-HAND PORTION OF SHEET 31

STA. 413+00

STA. 429+00